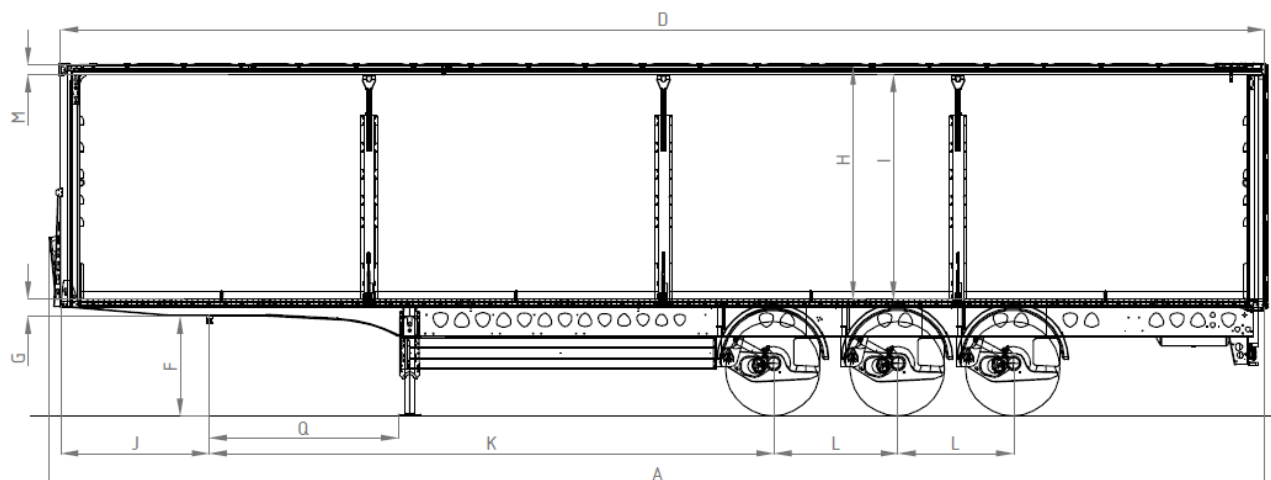


# SAPL 27 LTN

Semi-trailer with a lightweight steel construction and sliding tarpaulin



## Technical Data (basic version)

|          |   |               |            |
|----------|---|---------------|------------|
|          | <b>Tare weight</b>  | <b>4.982</b>  | <b>kgs</b> |
|          | Vertical load / technically possible                          | 15.000/17.000 | kgs        |
|          | Axle assembly load / technically possible                     | 24.000/27.000 | kgs        |
|          | Gross weight / technically possible                           | 39.000/44.000 | kgs        |
|          | Theoretical payload / technically possible                    | 34.018/39.018 | kgs        |
| <b>A</b> | Total length  | 13.850        | mm         |
| <b>B</b> | Vehicle width   | 2.550         | mm         |
|          | Total height unladen  | 4.000         | mm         |
| <b>D</b> | Loading length  | 13.620        | mm         |
|          | Loading width   | 2.490         | mm         |
| <b>F</b> | Coupling height (Attention: consider total height!)           | 1.130         | mm         |
| <b>G</b> | Frame neck height (frame height over wheel coupling)          | 190           | mm         |
| <b>H</b> | Internal height (floor to underside of cross member)          | 2625          | mm         |
|          | Lateral loading height (floor to underside of tarpaulin rail) | 2535          | mm         |
| <b>J</b> | Front overhang / front overhand radius                        | 1.680 / 2.040 | mm         |
| <b>K</b> | Wheel base  | 6.390         | mm         |
| <b>L</b> | Axle spacing  | 1.410 / 1.310 | mm         |
| <b>M</b> | Height of tarpaulin rail                                      | 112,5         | mm         |
|          | Loading width at rear (dimension between portal posts)        | 2.480         | mm         |
|          | Tyres   | 385/65 R 22,5 | 6x         |
|          | Spring track / wheel track                                    | 1.300 / 2.040 | mm         |
|          | Euro pallet capacity  | 34            | Pcs.       |
| <b>O</b> | Rear loading height, unladen (trailer in horizontal position) | 1.340         | mm         |

### Technical Description (Basic Equipment):

- Welded frame construction made of fine-grained steel S700, cross beam spacing (Z-profiles) approx. 375 mm, reinforced cross beams 3mm
- Mechanical landing gears 2 x 12 to
- SAF-brake system with disc brakes 370 mm; air suspension with raise/lowering device and operating valve at rear left
- Tires 6x, Goodyear, 385/65 R22.5, with steel rims
- EG- brake system type Knorr, mit Roll Stability Program (RSP), EBS 2S/2M, parking brake as spring brake with double release valve, aluminum compressed air tank, diagnostic capability via EBS socket ISO 7638
- 24 V lighting system, Hella, 2 x 7-pin and 1 x 15-pin plug socket according to DIN ISO 3731/12098, 2 multi-functional rear light systems (including rear fog lamp and reversing light), 2 clearance lights with rubber arms, 2 clearance lights (LED) on the front wall, 4 pairs of side marker lights (LED)
- Stable ecotrail-hybrid floor, GRP fabric on the underside as reinforcement and protection against moisture, floor sections tightly sealed on the outer frame, permissible forklift axle load 7,000 kg in accordance with DIN EN 283
- Body certified for general cargo in accordance with DIN EN 12642 Code XL, DIN EN 12195-1 and VDI 2700, for maximum payload
- Smooth front wall with high-strength sandwich panel, additional vertical and horizontal reinforcement profiles, front wall stanchions and reinforcement profiles made of S700 fine-grain steel, front wall panels riveted to front wall stanchions, 200 mm high protective strip integrated at bottom of the front wall panels, front wall bolted to the chassis
- Rear portal bolted to the chassis, rear portal stanchions made of S700 fine-grain steel, clear width between portal stanchions of 2,480 mm, rear doors complete with smooth outer surface and each fitted with 2 integrated power brace locks and stable closure mechanisms, door checks left/right, fold-up portal beam (System Berger), rubber buffer left/right
- 3 pairs of sliding stakes with lattice pockets for a total of 6 rows of lattices, support at bottom for 400 mm substitute side panel comprised of aluminum lattices
- 2 rows of aluminum side lattices, V-profiles, each 100 mm high
- Edscha Hybrid sliding tarpaulin, can be opened at both front and rear, braced by means of a 4-cable system
- 18 pairs of lashing rings flush-fitted in outer frame, certified according to DIN EN 12640 for tensile loading of up to 2,500 daN
- Multifunctional perforated outer frame, 68 hook-in possibilities at spacing of 187 mm along the complete length of the outer frame on each side, lashing points certified according to DIN EN 12640 for tensile loading of up to 2,000 daN, other drilled holes available for additional fixing options of cargo control systems and other applications
- Side tarpaulins, tarpaulin material 850–900 g/m<sup>2</sup>, with vertical and horizontal strap bracing, 20 direct tensioners on each side of the vehicle, tarpaulin tensioner with compact crank mechanism at the rear, user-friendly opening at front through fitted hook-in strip. Roof tarpaulin 650–670 g/m<sup>2</sup>
- Practical equipment with semi-circular mudguards with integrated spray suppression according to 91/226/EEC
- 1 pc. of shim with holder
- Extendable rear access ladder
- 2 x ECE 70 reflectors on the rear doors
- Underride guard and lateral protective according to regulations, each made of aluminium profiles
- Paintwork for chassis and landing gear, front wall panel (outer surface only), front wall frame as well as rear doors in an RAL colour according to the customer's request (no metallic or brilliant colours). Connection board on the front wall, lateral protective device including holder and landing gear cross-brace painted in RAL 9005 black, semi-gloss. Sliding stakes are KTL-plated (cataphoretic immersion painting), black; underride guard and intermediate plates in anodized aluminium. Profiles and inaccessible areas additionally protected with cavity wax.